

ORDINANCE NO. 09-911

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF BLACK DIAMOND, KING COUNTY, WASHINGTON, REPEALING THE EXISTING “BLACK DIAMOND HISTORIC VILLAGE CORE DESIGN STANDARDS AND GUIDELINES” AND REPLACING THEM WITH THE “CITY OF BLACK DIAMOND DESIGN GUIDELINES”

WHEREAS, on November 2, 2000, the Black Diamond City Council approved the “Black Diamond Historic Village Core Design Standards and Guidelines” which applied to residential, commercial and industrial development throughout portions of the city; and

WHEREAS, as part of its overall effort to update city plans, codes and development standards, the City Council directed that new guidelines be prepared to address not only residential, commercial and industrial development, but also master planned developments; and

WHEREAS, on June 18, 2009, the City Council adopted Ordinance 09-909, which repealed existing Title 18 of the Black Diamond Municipal Code and adopted a new Title 18 in its place; and

WHEREAS, Chapter 18.74 of the new Title 18 adopts the “City of Black Diamond Design Guidelines” by reference and requires development to comply with the guidelines; and

WHEREAS, on January 6 and 20, 2009, the City of Black Diamond Planning Commission conducted a public hearing on the proposed guidelines and voted to recommend their adoption by the City Council; and

WHEREAS, the City Council has studied this proposal at numerous study sessions and conducted a duly noticed public hearing on February 26, 2009; and

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF BLACK DIAMOND, WASHINGTON, DOES ORDAIN AS FOLLOWS:

Section 1. The existing “Black Diamond Historic Village Core Design Standards and Guidelines”, approved on November 2, 2000, are hereby repealed.

Section 2. The new “City of Black Diamond Design Guidelines” as shown in the copy attached to this ordinance as Exhibit A and hereby incorporated by reference are hereby adopted.

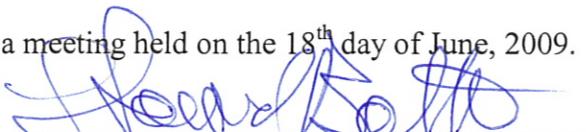
Section 3. This Ordinance shall be in full force and effect five days after its passage, approval, posting and publication as provided by law. A summary of this Ordinance may be

published in lieu of publishing the Ordinance in its entirety.

Section 4. If any provision of this Ordinance is determined to be invalid or unenforceable for any reason, the remaining provisions of this Ordinance shall remain in force and effect.

Introduced the 18th day of June, 2009.

Passed by a majority of the City Council at a meeting held on the 18th day of June, 2009.



Mayor Howard Botts

Attest:



Brenda Martinez, City Clerk

APPROVED AS TO FORM:

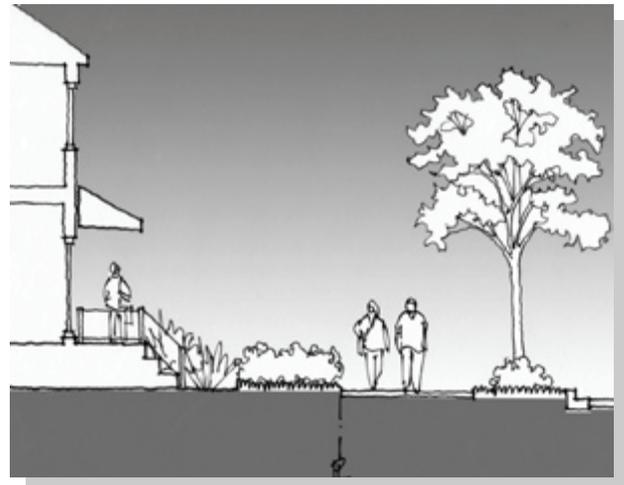
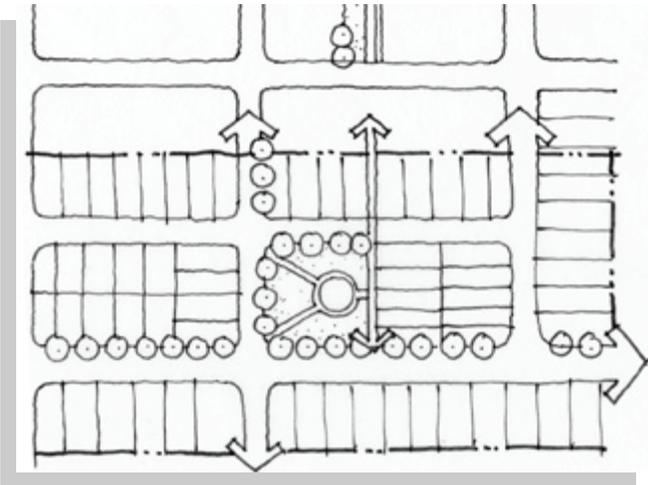
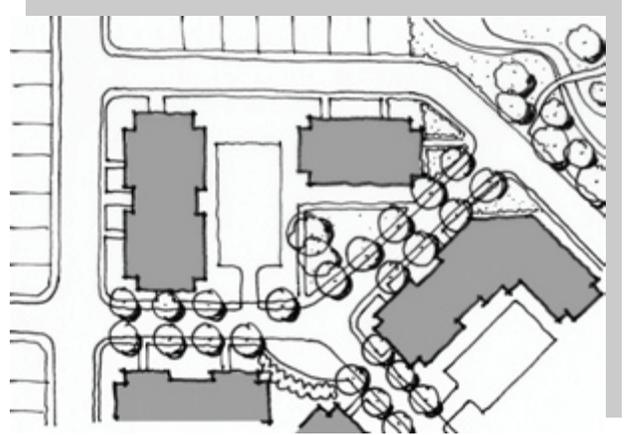
Loren D. Combs, City Attorney

Published: 6/23/09

Posted: 6/19/09

Effective Date: 6/28/09

BLACK DIAMOND DESIGN GUIDELINES
for
Multi-family Development



Adopted June 18, 2009

Introduction and Purpose

This section of the Design Standards and Guidelines focuses on site planning and design guidance for new multi-family development. The city's historic development patterns reflect several areas of growth and annexation. The residential neighborhoods of the historic town-site and Morganville are composed of small lots in traditional grid patterns and narrow streets. Other areas have developed in a non-grid pattern with larger lots. Now, with the rate of new development likely to increase significantly over the next 20 years, the city seeks to accommodate residential development with a mix of types, sizes and densities, clustered to retain the open space and rural land uses that form the area's natural beauty, and to connect housing, shopping, employment and recreation in a more efficient manner. With a maximum density of 12 units per acre, how new housing types such as townhouses, cottage housing and courtyard housing are designed will be crucial in ensuring a proper "fit" with the surrounding community. The design guidelines are organized in the three categories below.

Site Design

Site planning arranges building masses, open space, parking and circulation to create a site design that is orderly, visually pleasing, and that contributes positively to both the surrounding area and the development itself. Historic development patterns in Black Diamond relate to the street, encouraging people to participate more fully in their community. These site design guidelines will promote neighborhood compatibility, retention of natural features, integration with the surrounding community, opportunity for social interaction, and a safe, comfortable, and interesting environment for residents.

Building Design

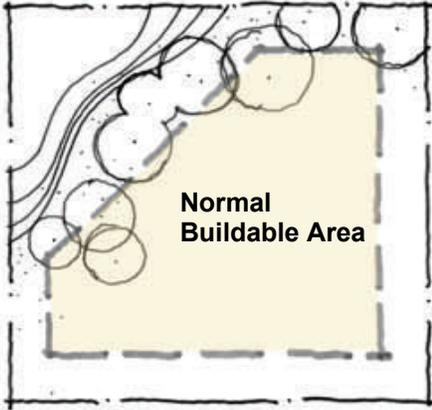
The building design guidelines address the overall external appearance of multi-family development, including building forms, details, and proportions. Use of single-family residential design elements are required to reduce perceived density, give character to the development and its individual dwelling units, add visual interest, and be compatible with the neighborhood context.

It is not intended that these guidelines prescribe one architectural style or a specific design character. There are various architectural styles found in Black Diamond's housing stock that help create unique settings. The primary focus should be to construct a high quality residential environment within the context of the existing community. Quality development is further encouraged through a sustainable design approach.

Landscaping

The rolling topography and variety of open pastures and meadows, wetlands and forested areas in the city help define its character. New developments can establish visual connections with these natural features through the choice and placement of landscape features. Moreover, an attractive, well-maintained, natural landscaped environment contributes to residents' quality of life and also enhances the appearance of the surrounding neighborhood.

SITE DESIGN



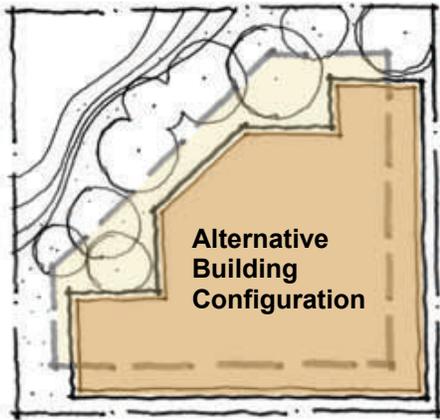
A. Context

Intent

Multi-family development successfully contributes to the overall community when relationships with the existing and planned land uses, development patterns, and context are considered.

Standards

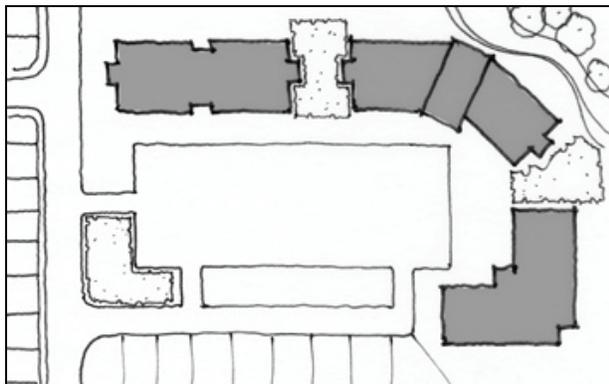
1. Developments shall comply with the City's tree preservation ordinance.



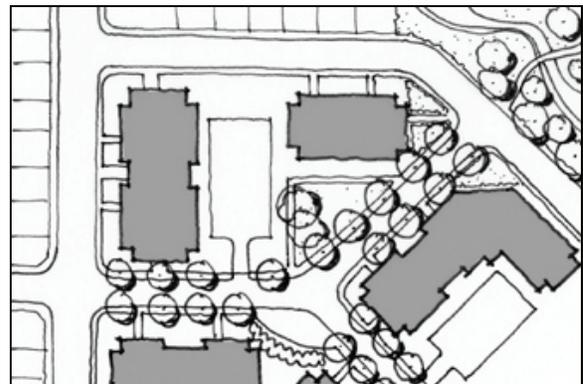
Guidelines

1. Incorporate existing unique site amenities such as views, mature trees, and similar natural features into developments whenever possible.
2. Use complementary building arrangements, buffers, and avoidance of incompatible building scale to ensure that new multi-family residential development is compatible with residential development in the immediate area. For example, cottage housing is encouraged in older, smaller Lot residential neighborhoods to complement similar siting in the area.
3. Complement existing landscape materials, location, and massing on adjacent developments with new landscape plantings.

Alternative site design can preserve mature trees and natural features



Undesirable Layout



Desirable Layout

Orient and cluster buildings, and design parking, landscaping and open space in ways that connect to surrounding neighborhoods, and complement view sheds and surrounding natural features.

SITE DESIGN

B. Building Siting

Intent

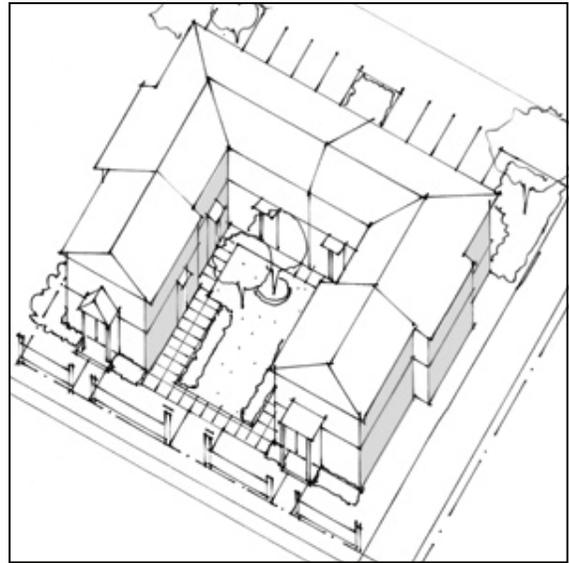
Appropriate building siting can reduce the perceived density of multi-family developments, maximize open space areas, provide “eyes on the street” surveillance, and enhance neighborliness by creating community gathering spaces.

Standards

1. Developments shall be oriented parallel to the public street, with setbacks from the street used to create landscaped open space.
2. Primary building entries shall be clearly identifiable and visible from the street, with well-defined walkways from pedestrian routes.

Guidelines

1. To create an appropriate transition from residences and the street, use the space between the building and sidewalk to provide security and privacy for residents and to encourage social interaction among residents and neighbors.
2. Cluster multi-family buildings around courtyards, gathering areas and open spaces.
3. Repeat these design elements throughout portions of the development that are not oriented to the street.

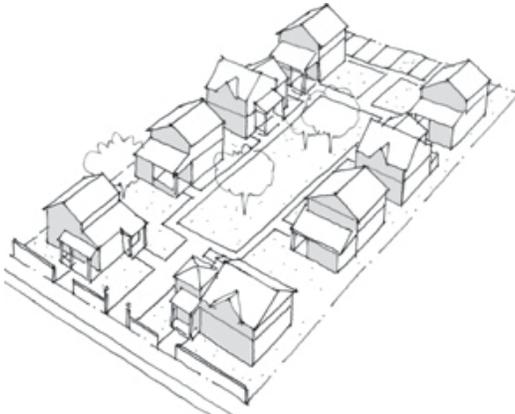


Buildings around courtyard and sited parallel to the public street with direct entrances and windows allowing “eyes on the street”

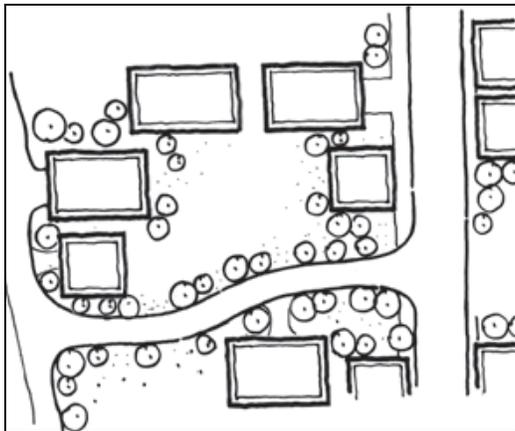


Appropriate transition from residence to street

SITE DESIGN



Centralized open space provides convenient access for many units



Large and small open spaces are connected and oriented to dwellings



Tot lot centrally located for safety and accessibility

C. Open Space

Intent

Common open space provides opportunities for casual social interaction and safe play areas for children, and it reduces the perceived density of the development. Private open space serves as an outdoor room for residents.

Housing Type	Private	Common
Cottage	Minimum 200 sf usable open space per unit separated from common open space by hedge or fence not to exceed 36" in height	Minimum 400 sf per cottage Minimum 20 ft. width At least 50% of cottage units abut common space
Courtyard	Minimum 200 sf usable open space per unit.	Minimum 300 sf per unit for courtyard space. Minimum 25 ft width At least 50% of units abut courtyard space
Townhouse	Minimum 300 sf usable open space per unit at ground level	

Guidelines

1. Conveniently locate open space to the majority of units for recreation and social activities.
2. Open spaces should be sheltered from the noise and traffic of adjacent streets or other incompatible uses. Take advantage of sun orientation to provide a comfortable environment.
3. Provide well-defined open space edges through the use of walkways, buildings or landscaping.
4. For larger developments, provide a series of connected open space areas of varying shape, appearance and intended use.
5. Require private open space (such as yard, patio or balcony) that is visible and can be entered from inside the dwelling for all units.
6. Define boundaries between private and common open spaces by elements such as low walls or plant

SITE DESIGN

C. Open Space

Guidelines *CONTINUED*

7. Site and design buildings so that windows of neighboring units do not overlook private open spaces.
8. Common outdoor spaces shall provide at least three of the following amenities to accommodate a variety of ages and activities to meet the needs of the residents:
 - a. Site furnishings (benches, tables)
 - b. Play areas (locate away from public streets)
 - c. Gardens
 - d. Patios or courtyards
 - e. Special paving, such as stone, brick or other unit pavers
 - f. Covered structure (i.e. gazebo)
 - g. Water feature



Locate shared amenities such as outdoor play areas and bicycle parking



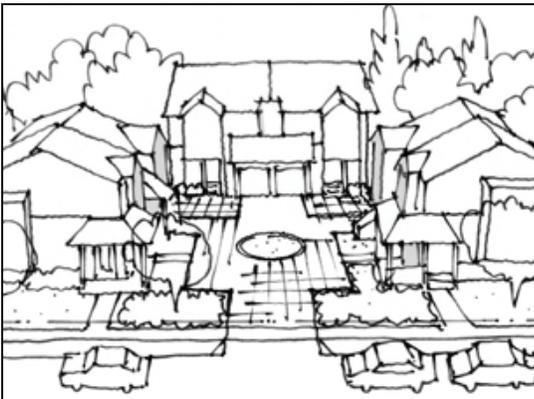
Private open space at rear of unit

SITE DESIGN

D. Parking/Vehicular Circulation

Intent

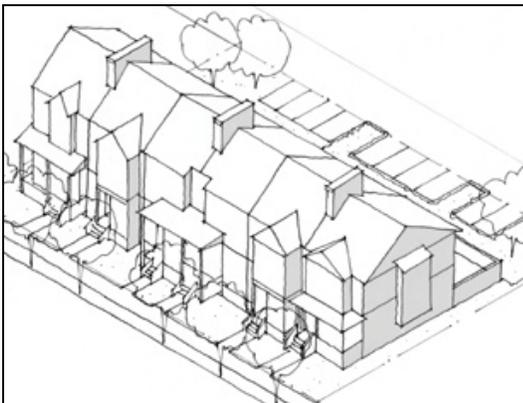
Safe and efficient circulation and parking arrangements take into consideration the needs of pedestrians, children at play, parking lot appearance, and safety.



Single curb cuts are preferred. Recessed garages and enlarged entries has a positive impact on the streetscape.

Guidelines

1. Parking areas shall be located in the development's interior and not along street frontages. Driveway openings along street frontages shall be limited to one per 150 lineal feet of street facing property.
2. The number of trees required in the interior landscape area in parking lots shall be dependent upon the location of the parking lot in relation to the building and the public right-of-way (street):
 - a. Where the parking lot is located between the building and the street, one street tree for every four spaces shall be provided (1:4)
 - b. Where the parking lot is located to the side of the building and partially abuts the street (max. 50%), one tree for every six spaces shall be provided (1:6)
 - c. Where the parking lot is behind the building and is not visible from the street, one tree for every eight spaces shall be provided (1:8)
3. Parking lot lighting shall be placed to create adequate visibility at night and evenly distributed to increase Security;
 - a. Control vehicle speeds by appropriate signage, changes in roadway texture, and other traffic calming devices, where necessary.
 - b. Avoid blank walls with rows of garage doors that face the public street front.
 - c. Use single-car garage doors, rather than double-car garage doors.
 - d. Provide landscaping around garages, and tuck-under parking.
 - e. Include an adjacent pedestrian entry path that connects to the public sidewalk along all entry drives. Provide elements that define the main entry, including lighting, textured paving, and accent plant



Rear yard parking is preferred.

SITE DESIGN

E. Pedestrian Connections

Intent

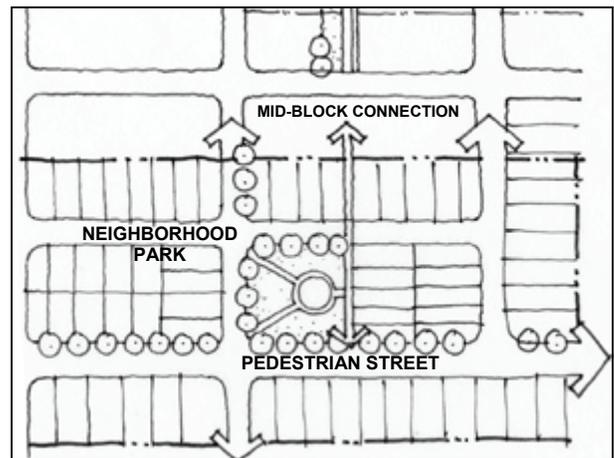
To create a network of safe, consistent, and convenient linkages for pedestrians, including locating building entrances adjacent to public sidewalks.

Standards

1. A comprehensive system of pedestrian walkways shall link all site entrances, building entries, parking areas and common outdoor spaces with the public sidewalk.
2. Clearly defined pedestrian connections shall be provided:
 - a. Between public sidewalks and building entrances when buildings are not located directly adjacent to the sidewalk.
 - b. Between parking lots and building entrances.
3. Pedestrian walkways shall be a minimum 5 feet of clear, unobstructed passage and the type and nature of all materials shall be consistent within a development.
4. Walkways shall be accessible to disabled persons and in conformance with the Americans with Disabilities Act

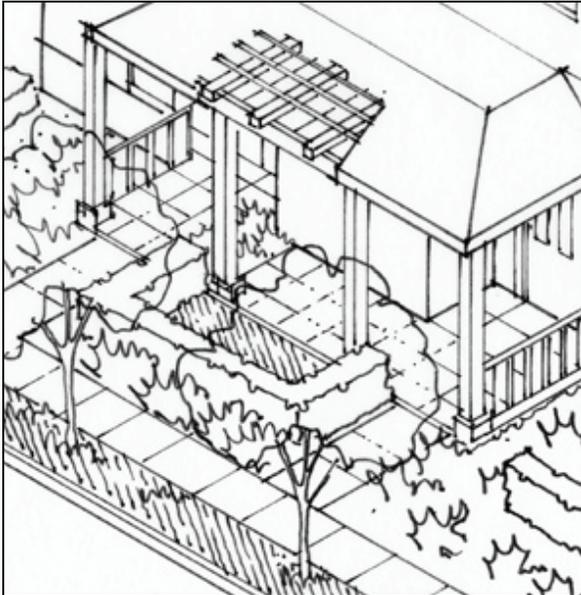
Guidelines

1. Provide pedestrian access to adjacent existing or planned open space areas and trails.
2. Avoid combining vehicle and pedestrian access to dwelling units as this does not allow adequate room for landscaping and other features that can personalize the front entry.
3. Locate walkways to minimize the impact of pedestrians on the privacy of nearby residences or private open space.
4. Provide a landscaped planting area between walkways and building facades.



Desirable connections to neighborhood amenities

SITE DESIGN



Porches, patios, walkways and covered trellises help identify individual entrances

F. Front Yards/Entrances

Intent

To provide separation between buildings and the public realm where the front yard serves as outdoor space and provides a welcoming and safe entry from the sidewalk to the building.

Standards

1. Primary building entries shall be clearly visible from the nearest public circulation walkway.
2. A minimum of 50% of the front yard shall be landscaped and planted.
3. Each individual unit shall have its own private walkway to the front door at ground level.
4. Walls, shrubs and other visual obstructions between the public realm and the building shall be limited to a maximum 3 ft height to allow easy surveillance.



Walkways combined with enhanced entry feature lead to each unit. Doors and windows encourage "eyes on the street"

Guidelines

1. A porch, covered stoop, or similar entry feature at each unit's front entry is strongly encouraged.
2. A wide range of plant materials, including perennials, lowering shrubs and native shrubs and groundcovers should be planted in the front yard. Plant material providing seasonal interest is encouraged.

BUILDING DESIGN

A. Building Mass

Intent

Reduce the apparent bulk of multi-family buildings and maintain a pedestrian scale compatible with Black Diamond’s small town character.

Standards

Cottage Housing		
Development Standards	By Right	Flexibility Available?
Minimum Lot Size	5000 sf	No
Maximum Lot Size	None	No
Maximum Lot Coverage (all impervious surfaces)	60%	No
Front yard Setback	Average of 10 ft, throughout the project; not less than 5 ft	Yes
Side Yard Setback	5 ft	No
Rear Yard Setback	10 ft	No
Maximum First Floor Area	50% of units, can not exceed 650 sf; 50% can not exceed 800 sf	Yes

Townhouse		
Development Standards	By Right	Flexibility Available?
Minimum Lot Size	7200 sf	No
Maximum Lot Size	Based on maximum number of units allowed	No
Maximum Lot Coverage (all impervious surfaces)	60%	No
Front Yard Setback	Minimum 10 ft Maximum 25 ft	Yes
Side Yard Setback	7 ft with 2 ft projections allowed for bay windows, chimneys, etc	No
Rear yard Setback	5 ft with an alley, 10 ft without	No
Maximum number of attached units per structure	6	yes

BUILDING DESIGN

A. Building Mass *CONTINUED*

Intent

Reduce the apparent bulk of multi-family buildings and maintain a pedestrian scale compatible with Black Diamond’s small town character.

Courtyard		
Development Standards	By Right	Flexibility Available?
Minimum Lot Size	14,400 sq ft, minimum of 4 units	No
Maximum Lot Size	Based on maximum number of units allowed (12)	No
Maximum Lot Coverage (all impervious surfaces)	60%	No
Front Yard Setback	Minimum 10 ft Maximum 25 ft	Yes
Side Yard Setback	5 ft	No
Rear Yard Setback	5 ft with an alley/10 ft without	No
Maximum number of attached units per structure	6	Yes

SITE DESIGN

B. Building Scale and Character

Intent

To help retain the city's small town character through building design.

Standards

1. Horizontal facades longer than 30 feet shall be broken down into smaller units, reminiscent of the residential scale of the neighborhood. At least three of the following methods shall be included:
 - a. roof forms such as gables and dormers
 - b. changes in materials
 - c. bays or projecting balconies
 - d. recesses/offsets
 - e. windows

Guidelines

1. Incorporate smaller-scale forms such as bays, recessed or projecting balconies, and dormers into the design to visually reduce the height and scale of the building and to emphasize the definition of individual units.
2. Where a neighborhood has a recognizable architectural context, such as Morganville, use those building features as visual cues for incorporation into the development's design.

Roofs

1. Gables facing the street are encouraged.
2. Vary roof lines within the overall horizontal plane through combinations of roof heights that create variation and visual interest.
3. Incorporate the roof pitch and materials of adjacent buildings into carport or garage roofs.



Roof pitch, dormers and change in upper level materials break down the scale of a multi-family development and relate to single family character nearby



SITE DESIGN

C. Facade Articulation

Intent

To avoid boxy and monotonous facades that lack human scale dimensions and have large expanses of flat wall planes.

Standards

1. Buildings shall include articulation along the facades facing and visible from public right-of-way. Modulation elements shall have a minimum 2-foot projection or recession from the facade, and be a minimum of 6 feet in length.

Guidelines

1. Use architectural treatments, such as recessed windows, moldings, decorative trim, and wood frames to add three-dimensional quality and shadow lines to the facade.
2. Windows of varied shape, size, and placement are encouraged.
3. Incorporate architectural detailing consistent with the development's overall design into garage doors, such as patterned garage doors, painted trim, or varied colors.



Building modulation



Bays, dormers, balconies and other projected or recessed design elements reduce the building's mass and add visual interest

BUILDING DESIGN

D. Building Entries

Intent

To create a socially and visually stimulating multifamily district with street level facades that support pedestrian activity.

1. Design courtyard entry gates as an important architectural feature of the building or development.
2. Emphasize and differentiate each individual unit's entry through architectural elements such as porches, stoops, or roof canopies, and detailing such as paint color, trim, materials or awnings.
3. Provide opportunities for residents to personalize individual entries by providing ground level space or a wide ledge for plants and other features.

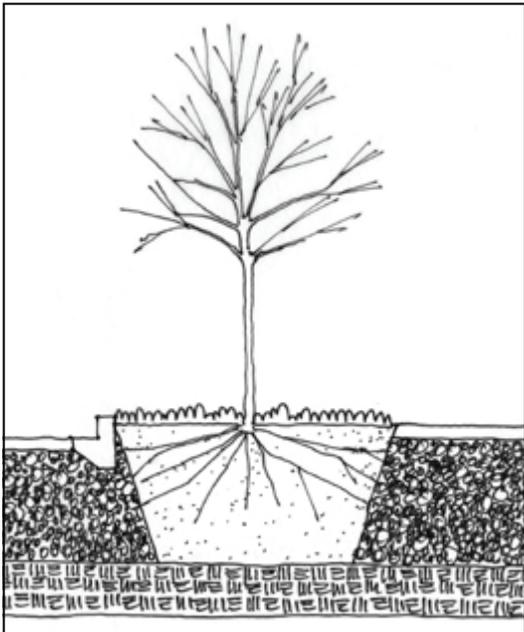


Low walls and landscaped areas help define the transition from public to private space. Ledges create opportunities to personalize the dwelling unit.



Well designed entry gate

LANDSCAPE DESIGN



Generous planting areas and uncompacted soil help sustain healthy landscaping

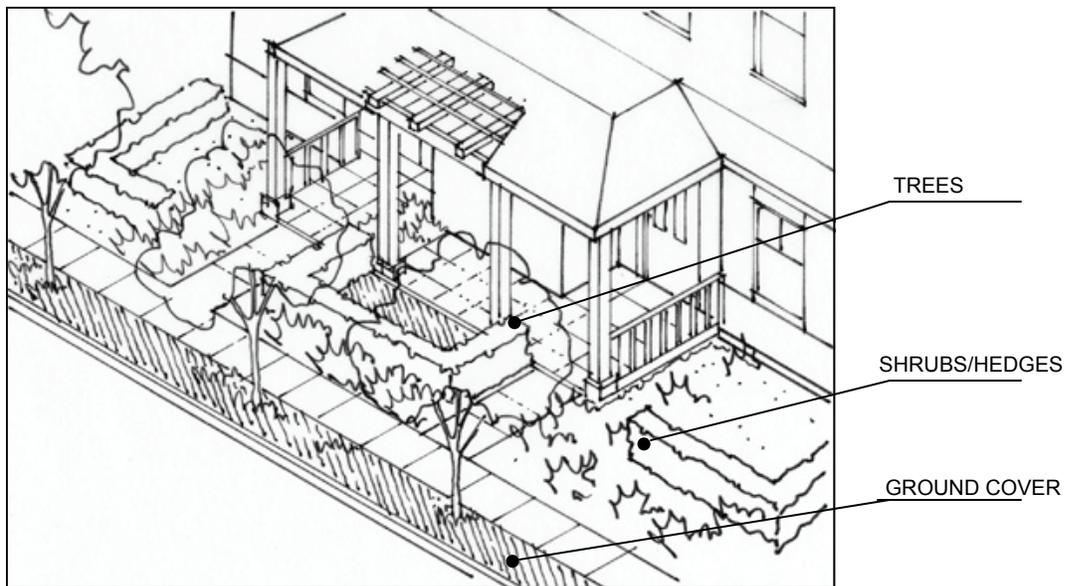
A. Landscape Design

Intent

To create unique identity, establish visual connections to surrounding natural areas, soften the architecture, provide shade and screen unattractive areas.

Guidelines

1. Use a three-tiered landscape planting approach consisting of ground cover; shrubs and vines; and trees.
2. Use different landscape design and plant materials in the various courtyards and common open space areas of the development to create an individual identity for each space.
3. Use landscape plantings to help define property lines and distinguish private space from public space through a change in plant material, form or height.
4. Select trees and shrubs based on their mature size and root characteristics. Plants with root systems that uplift hardscape materials or are considered invasive shall be avoided.
5. Sixty percent of plantings materials shall be a native species.

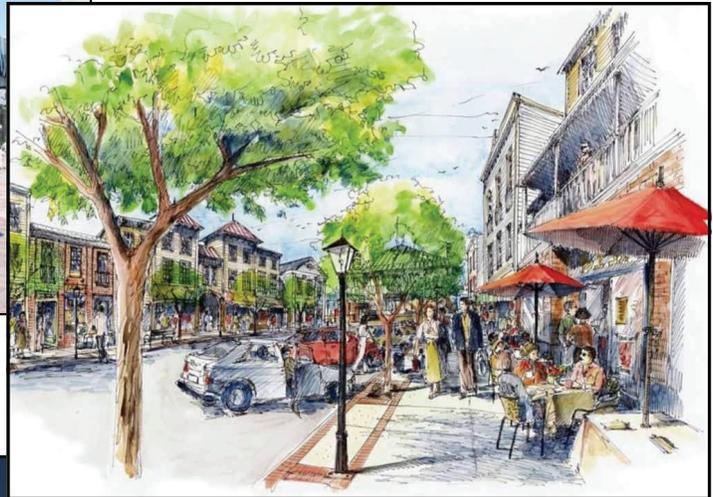


Landscaping to distinguish private space from public space

BLACK DIAMOND DESIGN GUIDELINES

for

Commercial Zones



Adopted June 18, 2009

Commercial Zones

Introduction and Purpose

This section of the Design Standards and Guidelines focuses on site planning and design guidance for the Commercial Zones of the city. These standards and guidelines serve to promote successful and vibrant commercial activity in the city while maintaining the rural and historic character that defines Black Diamond.

Site Design

Site design in Commercial Zones serves to orient buildings and transportation corridors in a way that allows for pedestrian and vehicular access while screening undesirable features such as trash areas and parking lots. A focus on landscaping and connectivity to adjacent uses will be stressed to meet the overall site design objectives.

Building Design

Building design in Commercial Zones will be compatible with the historic and rural context of the City of Black Diamond. These standards and guidelines ensure the objectives are met by addressing building design features individually to maintain pedestrian friendly and visually appealing commercial areas that cater to residents and visitors alike.

SITE DESIGN



Storefronts and entrances should enhance the activity on the street



Pulling back the entrance from the corner and providing additional sidewalk and landscaping

A. Orientation to the Street

Intent

The life of the commercial district is closely tied to the character of its public space. Collectively, buildings configure and shape the streetscape and other open spaces. New buildings should be located towards the street, near the sidewalk to promote community commercial activity, and to heighten the presence and improve the identity of commercial areas.

Guidelines

1. Locate and orient buildings to define public streets and civic spaces. Locate buildings close to the sidewalk and limit gaps to the minimum necessary to accommodate parking and access. Allow larger developments to pull back from the street edge for plazas and entry fore-courts. Plazas, courtyards and other pedestrian space shall include at least three of the following:
 - a. Special landscape
 - b. Pedestrian scale bollards
 - c. Accent lighting
 - d. Special paving, such as colored/stained concrete, brick or other unit paver
 - e. Seating, such as benches, tables or low seating walls
 - f. Water feature
2. Provide clear, recessed main building or shop entrances that do not interrupt street and/or retail continuity.
3. Set buildings back at the corners of inter-sections from the corner property lines at the ground floor to allow for both a more generous sidewalk and additional street landscaping to support a high level of activity and visibility.

SITE DESIGN

A. Parking Lots

Intent

To locate parking in a manner that is as visually unobtrusive as possible and provides safe, convenient access without detracting from the pedestrian environment.

Guidelines

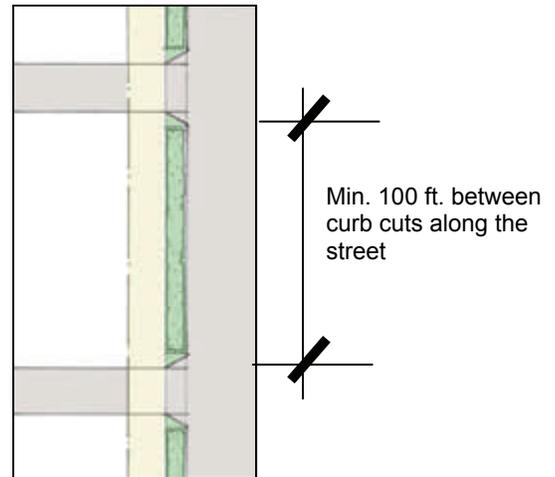
1. Parking lots shall be located behind or to the side of buildings.
2. Parking lots shall not abut street intersections (corner lots).
3. Parking lots located beside buildings shall be screened from the right-of-way with one or more of the following treatments:
 - a. Landscaping to sufficiently screen the first 3 feet in height.
 - b. Low walls made of decorative concrete, masonry, or other similar material and not exceeding a height of 3 feet.
 - c. Raised planter walls planted with a minimum of 80% evergreen shrubs not to exceed 3 feet in height.
4. Reduce visual impact of surface parking lots.
 - a. Where the parking lot is located to the side of the building and partially abuts the public right-of-way, one tree for every six spaces shall be provided;
 - b. Where the parking is behind the building and not visible from the public right-of-way, one tree for every eight spaces shall be provided.

note: no planting strip shall be less than 3 feet wide; 6 feet if trees are included

5. Enhance pedestrian safety by reducing and consolidating driveways, while providing for adequate access:
 - a. Distance between curb cuts should not be less than 100 feet.
 - b. The sidewalk pattern and material shall continue across the driveway.
6. Adjacent developments shall make every effort to share driveways.



Parking lot screened with low walls and landscaping



Pedestrian connections shall be maintained in larger parking lots

SITE DESIGN



Connections provide safety and comfort through large sites

B. Pedestrian Connections

Intent

The commercial district should connect through a network of public spaces and paths. New development should improve the pedestrian environment by making it easier, safer, and more comfortable to walk between businesses, to the sidewalk, to transit stops, and through parking lots.

Guidelines

1. Pedestrian connections shall be clearly defined with a combination of two or more of the following elements:
 - a. 6 inch vertical curb in combination with a raised walkway.
 - b. trellis, special railing, bollards, and/or other architectural features to accent the walkway at key points.
 - c. continuous landscape area minimum 3 feet wide (6 feet minimum if trees are included) on at least one side of the walkway.
2. Provide adequate building lighting at entries and along all walkways and paths through parking lots.
3. Pedestrian walkways shall include clear sight-lines to building entrances and shall not be less than 4 feet wide.



Clear sight-lines to entrances and public areas

SITE DESIGN

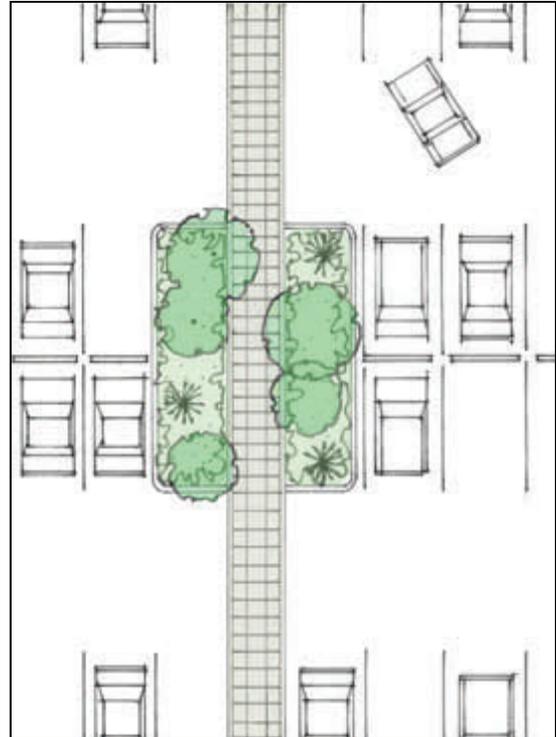
Pedestrian Connections *continued*

Guidelines

4. On-Site Pedestrian Circulation
 - a. Pedestrian paths or walkways shall be provided connecting all businesses and the entries of multiple buildings on the same development site.
 - b. Pathways and crosswalks shall be provided through parking lots along every fourth parking aisle or at intervals of not less than 150 feet.
 - c. Contrasting surface materials shall be used for pathways to delineate pedestrian areas from travel lanes.
5. Site Integration with Transit Facilities

Transit stops shall be integrated into the planning of adjacent site improvements.

 - a. Extra space for waiting areas shall be provided with benches, newspaper and bicycle racks, trash receptacles, a clock, bus schedule. Pavement for expanded waiting areas may be located within required setbacks and may count as landscape areas.
 - b. Integration of transit shelter into the building by providing overhead weather protection (building canopy) with appropriate height and depth, and lean bars affixed to the base of the building.



On-Site Pedestrian Circulation



SITE DESIGN



C. Screening of Trash and Service Areas

Intent

To reduce the impact of service, loading, and trash/recycling areas.



Guidelines

1. Service, loading and trash/recycling collection areas shall be screened from public view with solid evergreen plant material or architectural treatment similar to the design of the adjacent building.
2. Chain link is prohibited.

Note: alternatives may be considered if designs meet the intent and are approved by the Community Development Director



Examples of trash screening areas

BUILDING DESIGN

A. Massing

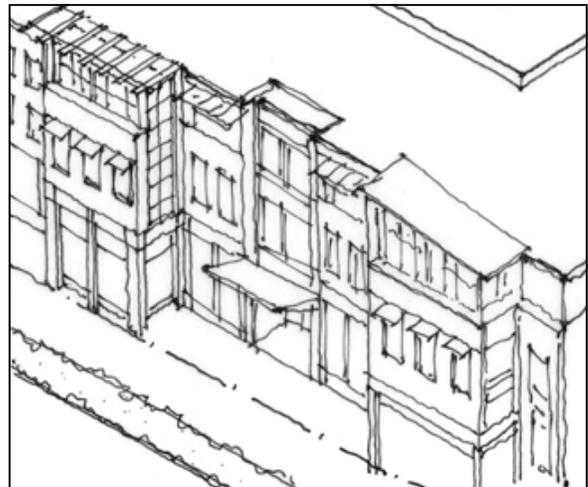
Intent

Give proper consideration to the scale and massing of individual buildings to be compatible with Black Diamond's desired architectural scale and character.

1. Design building volumes appropriate to the scale of their surroundings. In general, break down the scale and massing of larger buildings.
 - a. All new buildings with a wall of 60 lineal feet or more, facing a public right-of-way, shall create a 5 ft. minimum jog in those facades visible from the public right-of-way every 30 feet.
2. Articulate the facade with smaller windows, bays and other projected or recessed design elements as a transition in size to adjacent historic or residential structures that are smaller in scale.
3. Express rooflines as an extension of the building's composition:
 - a. Use a strong, detailed cornice or parapet in conjunction with a flat roof. Modulate the roof line at least every 60 feet.
 - b. Gable, hipped or shed roofs with a slope of at least three ft vertical to twelve ft horizontal are acceptable.
 - c. Other roof forms, such as arched, vaulted, dormer, or saw-toothed may be acceptable provided they don't exceed 60 feet without a change in plane or design.
 - d. Mechanical equipment shall be screened from the public right-of-way by an extended parapet wall or other roof form.



Buildings should reference Black Diamond's small, rural character



Example of building facade modulation standard



Gabled roofline with differentiated materials to articulate

BUILDING DESIGN



In this case, the “top” breaks up the perceived bulk of the building

B. Articulation

Intent

To reduce the apparent bulk of multi-story buildings and maintain a pedestrian scale in Commercial zones.

Guidelines

A “base, middle and top” shall be delineated for buildings 25 feet or higher.

1. Create a “base” at ground level using articulation and materials.
2. The “top” of the building will emphasize a distinct profile or outline with elements such as a projecting parapet, cornice, or pitched roofline. In addition, building facades abutting the street shall have an upper level setback at a height 25-50 feet above grade, with a minimum depth of 10 feet.
3. The “middle” of the building may be distinguished by a change in materials or color, windows, balconies, setbacks and signage.

BUILDING DESIGN

C. Ground Level Details

Intent

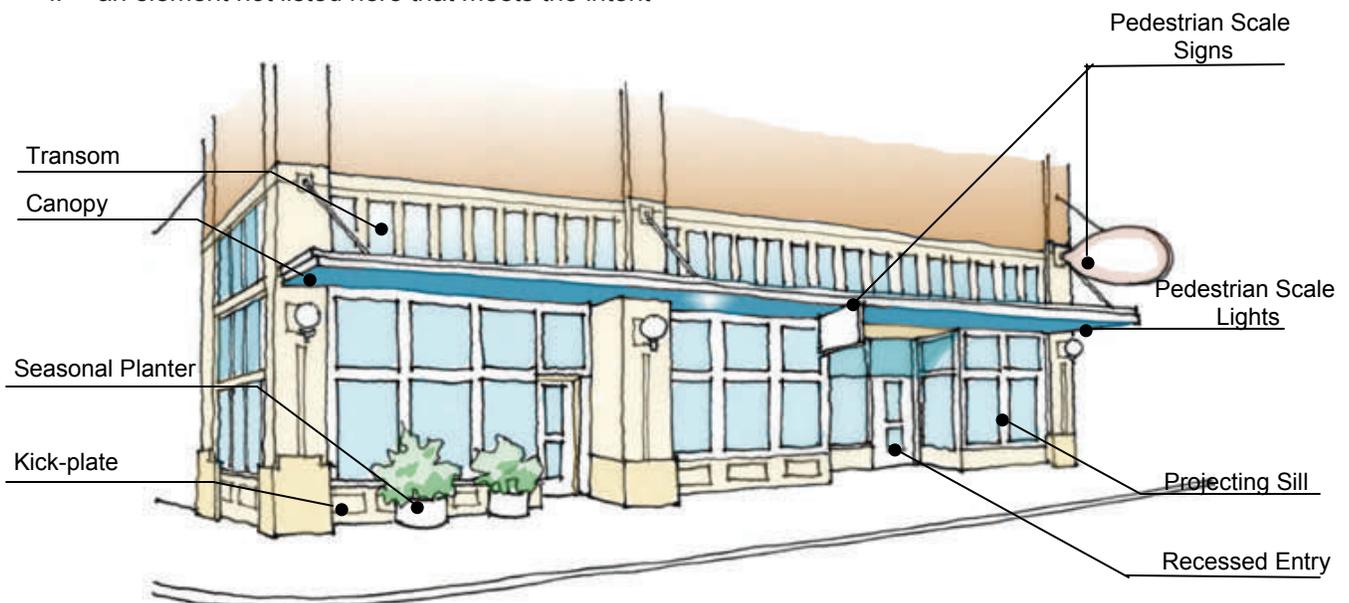
To create a socially and visually stimulating commercial district with street level facades that support pedestrian activity.

Guidelines

1. Promote active, pedestrian-oriented uses with a high degree of transparent window area. Reflective, opaque and highly tinted glass is prohibited.
2. Reinforce the character of the streetscape with the greatest amount of visual interest placed along the ground level of buildings. Facades of commercial and mixed-use buildings shall be designed to be pedestrian friendly through the inclusion of at least three of the following elements:
 - a. kickplates for storefront windows
 - b. projecting window sills
 - c. pedestrian scale signs
 - d. recessed entry
 - e. canopies
 - f. transom windows
 - g. pedestrian scale lighting
 - h. planters and/or seasonal hanging baskets supported by ornamental brackets
 - i. an element not listed here that meets the intent



Good pedestrian oriented ground level detailing improves the street life of a commercial street



BUILDING DESIGN



Historic mural work can fit with the character of a commercial area and break up blank walls



D. Blank Wall Treatments

Intent

To reduce the visual impact of blank walls by providing visual interest.

Guidelines

1. Blank walls longer than 30 feet facing streets or visible from residential areas shall incorporate two or more of the following:
 - a. Vegetation, such as trees, shrubs, ground cover And/or vines adjacent to the wall surface;
 - b. Artwork, such as bas-relief sculpture, murals, or trellis;
 - c. Seating area with special paving and seasonal plantings;
 - d. Architectural detailing, reveals, contrasting materials or other special visual interest.

BUILDING DESIGN

E. Corner Lots

Intent

To promote civic identity through prominent building features at visible locations.

Guidelines

1. Buildings located at corners of blocks, should be given significant architectural expression in the facade, roof form, massing and orientation, such as:
 - a. tower forms
 - b. peaked roofs
 - c. larger entrances



A significant corner building with unique architectural form can mark entry into the commercial area



BLACK DIAMOND DESIGN GUIDELINES

for

**Residential Uses
in the Historic Village Core**



Adopted June 18, 2009

Introduction and Purpose

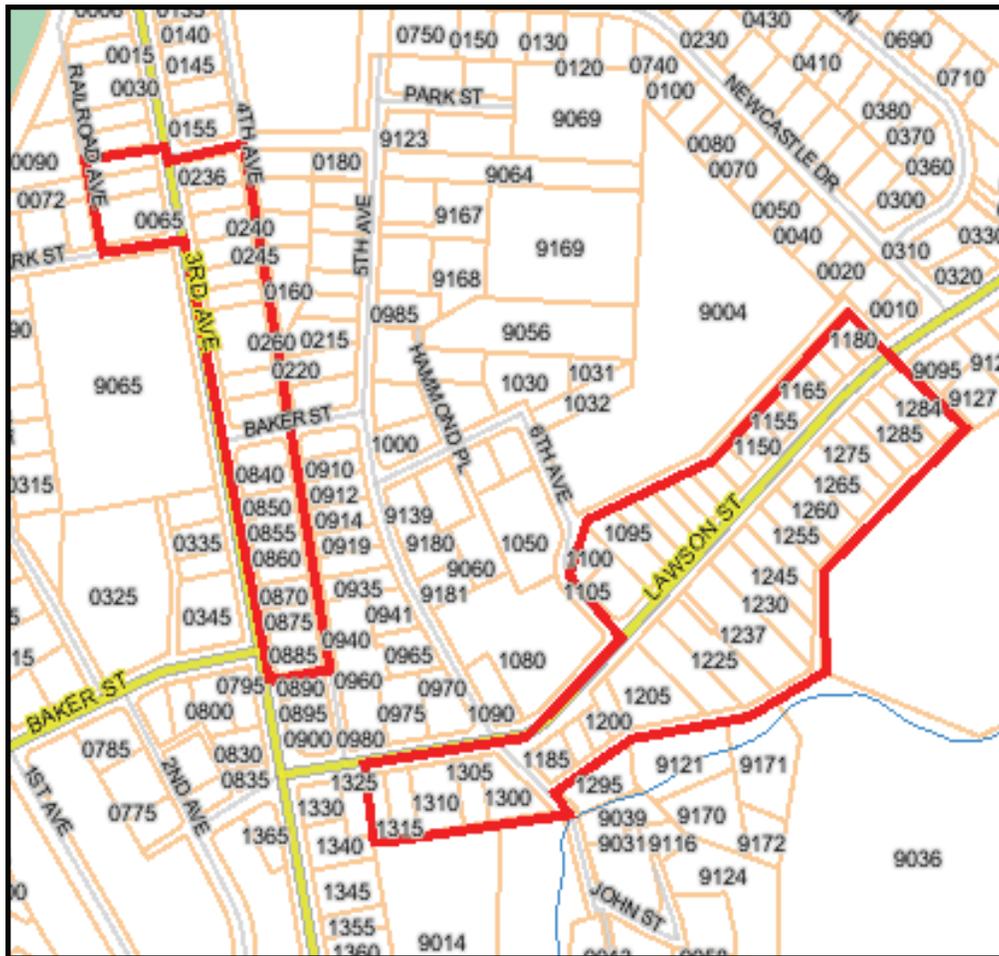
These guidelines are intended to guide infill development within the Historic Village Core as defined on the attached map and to all other properties identified by the 1998 Historic Resource Inventory prepared by the King County Office of Cultural Resources, in order to reflect the late 19th century vernacular characteristic of these areas. These guidelines shall apply to all new construction and exterior remodels and/or additions to existing homes built prior to 1922, when the cost of the proposed improvements exceeds 50% of the assessed value of the house only (not including land value). Properties occupied by post-1922 construction located within these areas are not required to comply with these guidelines.

The guidelines are intended to encourage the preservation and restoration of historic homes within the city, recognizing that they are valuable community assets (both economically and aesthetically), to the city and its citizens.

The most common types of homes historically constructed in these areas are commonly described as “company cottages” or “vernacular” style. Homes were small in size, with standardized configuration. Although some homes are two story, most were built as one story with an attic and a large front porch. The material used was wood, usually over a stone foundation.

The areas impacted by these regulations are as shown on the following pages.

Historic Residential Area Map



SITE DESIGN

A. Orientation to the Street

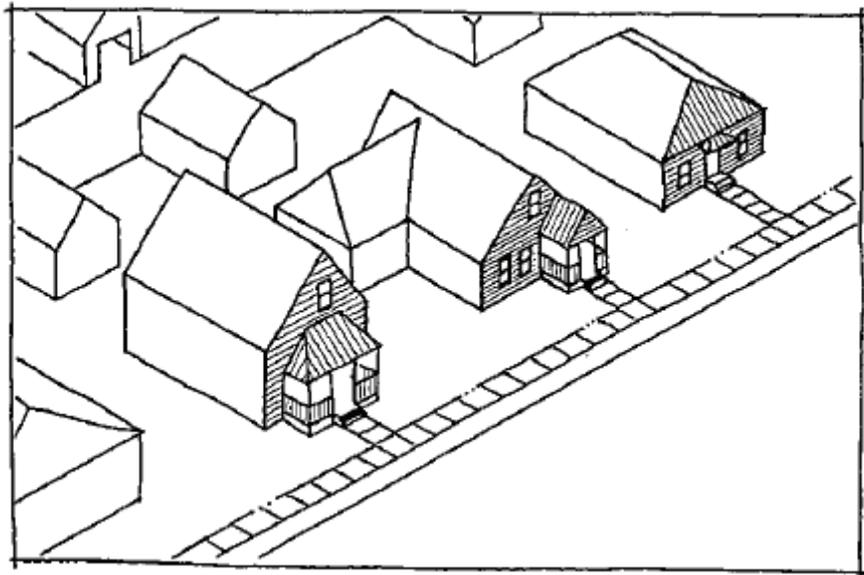
Intent

Historic homes are typically oriented to the street, sited to make their entries and use clear to neighbors and visitors. These features provide for a pleasant streetscape that enhances pedestrian access and walking, promoting interaction among neighbors.

Guidelines

Provide a front façade facing the primary street, siting a structure to make its entry and residential use clear to approaching visitors.

1. Require vehicular access from alleys where alleys exist.
2. Street facing garages should be architecturally compatible with and never dominate a residence.
3. Locate driveways no closer than 3 feet to side property lines to provide room for landscaping.
4. For new buildings that project beyond the footprint of homes on adjacent lots:
 - a. Limit the length and height of the projection into the rear yard area to reduce the impact of neighboring rear yards;
 - b. minimize windows, decks and balconies overlooking neighboring yards or screen in order to protect privacy.



Building facades should relate to the street

SITE DESIGN

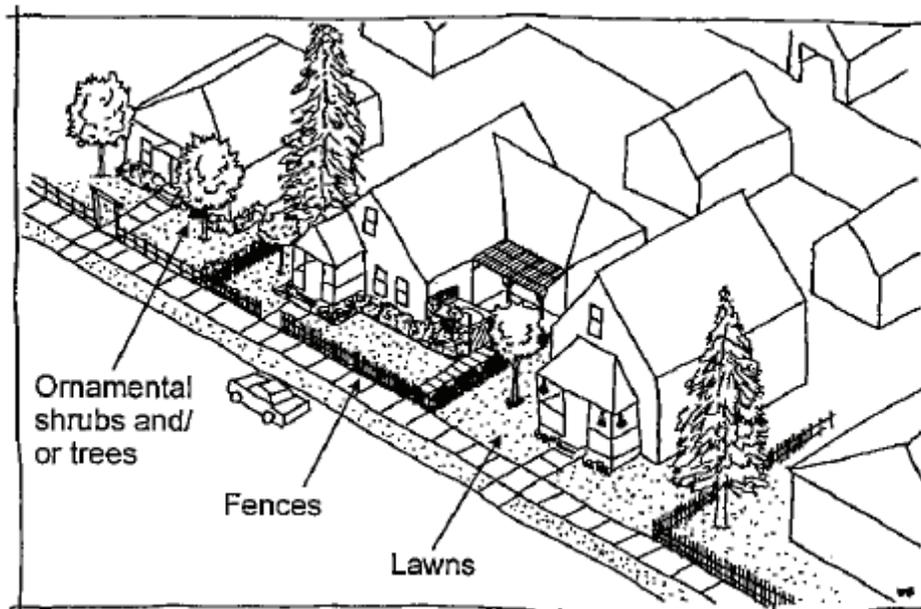
B. Fences and hedges

Intent

Prevent the installation of intrusive, nontraditional fences and tall hedges that cut the views of structures from the street.

Guidelines

1. Limit the height of front yard fences to no greater than 36 inches.
2. Use traditional style materials such as wrought iron or wood pickets in lieu of chain link fencing in front yards.



Desirable front yard treatment, including fences.

BUILDING DESIGN

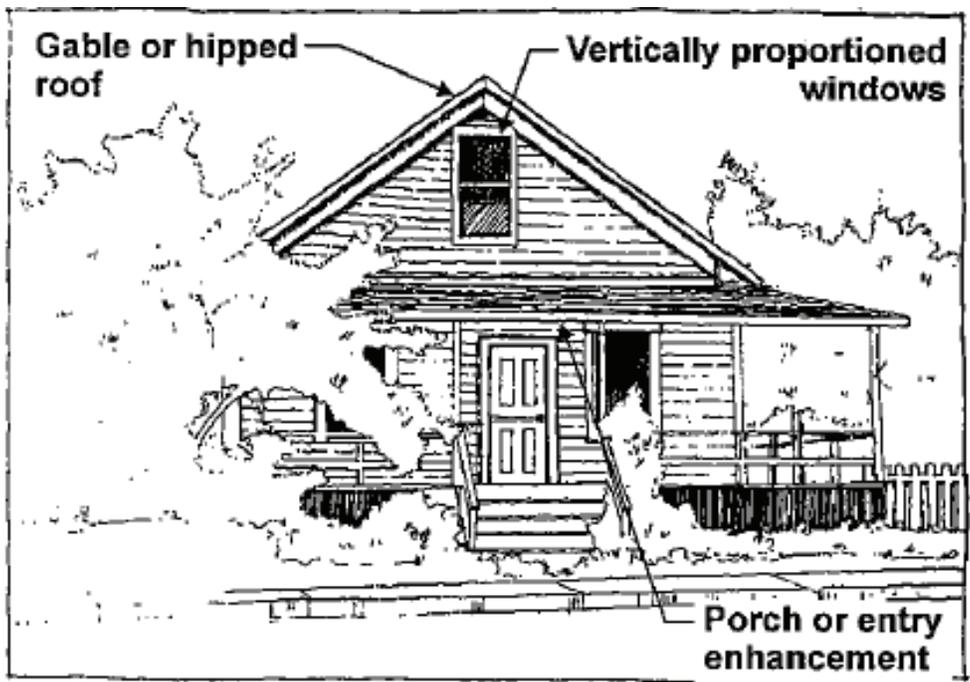
A. Building Facades

Intent

To provide building facades that feature traditional building elements and details, such as gabled roofs, porches, dormers and cornice lines, that add visual interest and reduce apparent bulk and scale..

Guidelines

1. Include the following features in new residential building facades:
 - a. gable or hipped roof;
 - b. porches or entry enhancement, such as an overhang or a sidelight;
 - c. vertically proportioned windows;
 - d. eaves.
2. Design principal building entries to be visible from the street and directly accessible from the public sidewalk.
3. Accent principal entries by building elements (stairs, roofs, special fenestration, etc.) and elevation at least 18 inches above the grade level of the sidewalk or street.
4. Provide a recess, porch, portal or other protected exterior area that encourages human activity.
5. Retain existing historic doors or replace with doors that match historic doors in materials, size and style, series of panels and same dimension frames.



BUILDING DESIGN

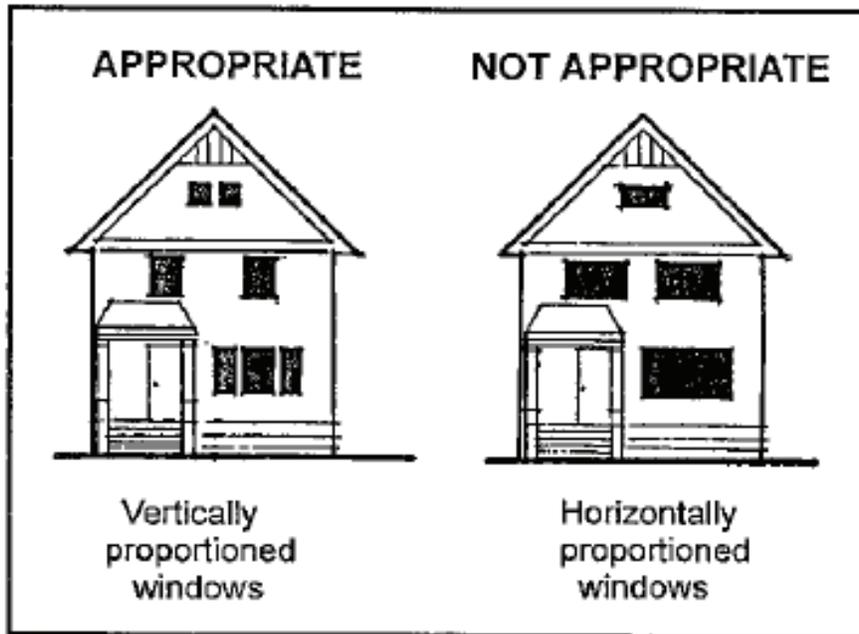
B. Windows

Intent

Provide traditional-oriented windows that highlight and accent a structure. Windows are vital elements of historic homes and are typically highlighted or accented. Historic window frames are wood, surrounded by a thick sash which accents the window, while protecting it from wind and rain.

Guidelines

1. Reflect the fenestration patterns of the neighborhood in new construction.
2. Use vertically proportioned windows in new construction. Prohibit the use of horizontally proportioned windows. Grouping of vertically proportioned windows is acceptable.
3. For remodeling of existing structures, replace windows facing the street with new windows of similar style and proportions.



SITE DESIGN

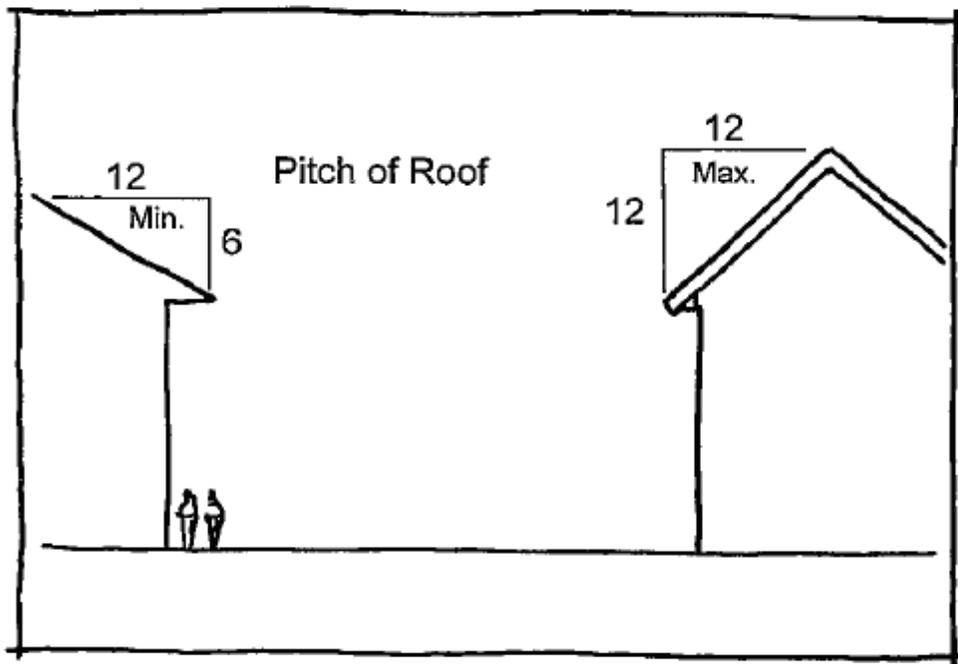
C. Rooflines

Intent

To add visual interest to a building and the street and complement neighboring structures by providing prominent roofs.

Guidelines

1. Roof designs shall incorporate at least one of the following:
 - a. gable or hipped roof;
 - b. broken or articulated roofline;
 - c. prominent cornice or fascia that emphasizes the top of the building;
 - d. other elements that emphasize a building's concept.
2. Create prominent pitched roofs with minimum slope of 6:12 vertical to horizontal ratio and maximum 12:12 ratio.
3. Allow different roof pitches for accessory structures not visible from the street.
4. For historic buildings, replicate the original shape or form of the roof in remodels or additions, unless the character of the original roof is not consistent with the type or style of roof of neighboring buildings.



BUILDING DESIGN

D. Exterior materials and colors

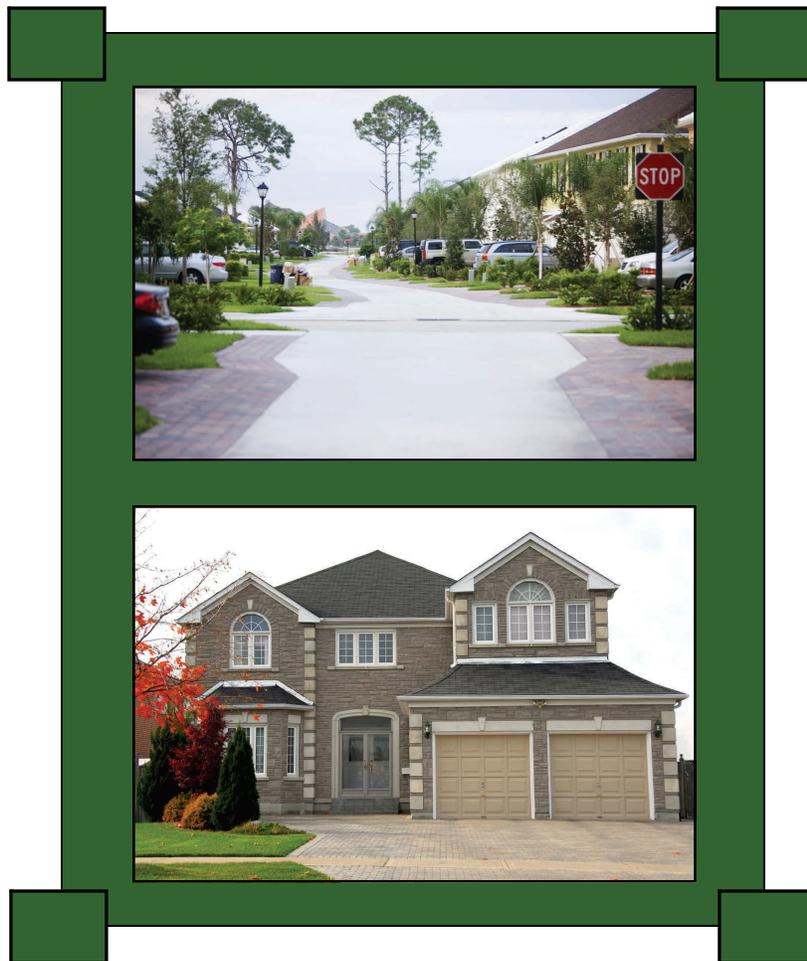
Intent

Ensure that exterior materials relate to typical historic patterns and add visual interest, and are of durable, high quality and easy to maintain.

Guidelines

1. Use horizontal wood siding (4 to 6 inch shiplap, clapboard, or wood shingles) or a composite equivalent.
2. Use compatible materials (wood siding or brick) for detached garages.
3. Use muted colors for the background color of buildings, with brighter/lighter colors being appropriate for trim.

BLACK DIAMOND DESIGN GUIDELINES
for
**Master Planned Development
Framework Design Standards & Guidelines**



Adopted June 18, 2009

Introduction and Purpose

The Master Planned Development (MPD) Framework Design Standards and Guidelines are intended to provide guiding principles for the overall design of MPD applications within the City. These guidelines are to be followed in consideration of an MPD at both the initial and subsequent phases of approval. It is anticipated they will be supplemented by additional guidelines and standards that are developed when more specific plans for phased development are proposed. Those guidelines may be initially drafted by the MPD developer for consideration by the City prior to eventual adoption as part of a development agreement. As such, these guidelines are not intended to address all potential aspects of future development, but to provide an overall framework upon which additional guidelines may be added to in the future.

The more specific guidelines that are included at this time reflect important issues to the community which need to be carried forth in future amendments.

The statements contained herein are intended to be standards and guidelines, rather than prescriptive rules, and thereby provide an amount of flexibility. Any decision regarding strict application of any guideline contained herein will be made by the City Council as part of its consideration of granting overall MPD approval.

General Principles & Site Planning

3

GENERAL PRINCIPLES AND SITE PLANNING

A. Environmentally Sustainable

Intent

To provide resource-efficient site design which includes consideration for saving trees, constructing on-site stormwater retention/infiltration features, and building orientation to maximize passive solar heating and cooling.

Guidelines

1. Implement a construction waste management plan to reduce construction waste. Consider life-cycle environmental impacts of building materials.
2. Incorporate energy saving techniques into all aspects of building's design and operation.
3. Maximize water conservation by maintaining or restoring pre-development hydrology with regard to temperature, rate, volume and duration of flow; use native species in landscaping; recycle water for on-site irrigation use.
4. Use measure that can mitigate the effects of potential indoor air quality contaminants through controlling the source, diluting the source, and capturing the source through filtration.
5. Reduce overall community impacts by providing connectivity from the project to the community; by incorporating best management practices for stormwater management; by creating useable public spaces such as plazas and parks; and by protecting important community-identified viewsheds and scenic areas.
6. Grading plans shall incorporate best management practices with phased grading to minimize surface disturbance and to maintain significant natural contours.

General Principles & Site Planning

B. Using Open Space as an Organizing Element

Black Diamond has a specific history and setting that involves varied topography, an agricultural past, forested areas, mining, and a small town scale. Care should be taken to reflect these patterns in master planned developments. In addition, the MPD chapter of Black Diamond’s Municipal Code requires that fifty percent (50%) of the total land area of an MPD be maintained as open space. Proper design and integration of this open space into a development is very important.

Guidelines

1. All master planned developments shall include a wide range of open spaces, including the following:
 - a. Sensitive environmental features and their buffers
 - b. Greenbelts
 - c. Village greens
 - d. Parks and school playgrounds
 - e. Public squares
 - f. Multi-purpose trails

These features should be deliberately planned to organize the pattern of development and serve as center pieces to development cluster, not merely as “leftover” spaces.

2. Open spaces shall be linked into an overall non-motorized network through sidewalks, trails and parkways. The overall network shall be delineated at initial MPD approval and implanted through subsequent plats and permit approvals.
3. Stands of trees as an element of open space.

Due to the propensity of severe wind events in the Black Diamond area, an MPD should incorporate the preservation of larger rather than smaller stands of native trees.



General Principles & Site Planning

5



C. Integrating Development with Open Spaces

Intent

To allow for an efficient use of land, lower the cost of infrastructure and construction, protect environmentally sensitive areas, and maintain a small town “village” character within an MPD. Development is to be integrated with networks of preserved natural features and developed open space for both passive and active recreational uses.

Guidelines

1. Use of conventional, suburban-style subdivision design that provides little common open space shall be avoided.
2. Groupings of primarily residential development of approximately 400-600 units should be contained generally within a quarter mile radius to support walking, bicycling and future transit service. Development clusters shall be surrounded by a network of open space with a variety of recreational uses (including trails) to provide connections between clusters.
3. Methodology for Planning Development in clusters.
 - a. environmentally sensitive areas to be protected (including streams, wetlands, steep slopes, wildlife corridors, and their buffers) shall be identified, mapped and used as an organizing element for design;
 - b. areas for development of housing and commercial development shall be indicated;
 - c. streets and public spaces (as well as sites for public facilities such as schools, fire stations and other civic structures) shall be identified;
 - d. lots and groups of lots with various ownerships (i.e. fee simple by occupant, condominium, single ownership apartments, etc) shall be integrated with one another throughout all phases of a project;
 - e. views of Mt Rainier and other desirable territorial views shall be identified and integrated into site planning to maximize viewing from public spaces (streets, trails, parks, plazas, etc.).

General Principles & Site Planning

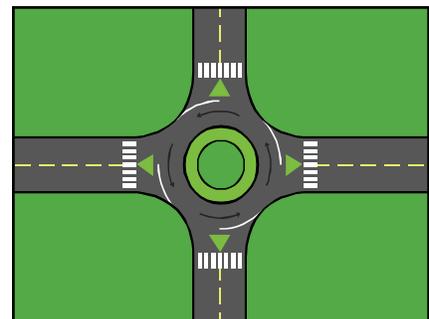
D. Ensuring Connectivity

Intent

To promote ease of mobility and access within all portions of the development.

Guidelines

1. Pedestrian Connectivity
 - a. Similar to a traditional small town, services and common spaces shall be easily accessible to residents on foot. Off-street pedestrian trails are to be provided as a network throughout the development. Pedestrian connections shall be provided where cul-de-sacs or other dead-end streets are used.
2. Street Connectivity
 - a. The system of streets shall demonstrate a high degree of both vehicular and pedestrian connectivity, allowing residents and visitors multiple choices of movement. Isolated and dead-end pockets of development are not desired.
 - b. Cul-de-sacs shall be avoided unless there are no other alternatives.



General Principles & Site Planning

E. Mixing of Housing

Intent

To encourage a diversity of population and households within Black Diamond through a range of choices in housing types and price.

Guidelines

1. MPD's shall include various types of housing, such as:
 - a. Single Family, detached, on various sized lots
 - b. Single Family, attached:
 - duplexes
 - townhouses (semi-attached)
 - row houses (attached, common walls)
 - courtyard houses
 - c. Cottage housing
 - d. Apartments
 - e. Accessory Dwelling Units
2. Each cluster of development shall include a variety of unit types and densities.
3. For Single Family developments, alley access to garages is desired. Direct driveway access to streets should only occur if there are no other alternatives.
4. Large apartment complexes and other repetitive housing types are discouraged. Apartments should replicate features found in Single Family Residential areas (i.e. garages associated with individual units, individual outdoor entries, internal driveway systems that resemble standard streets, etc.).



General Principles & Site Planning

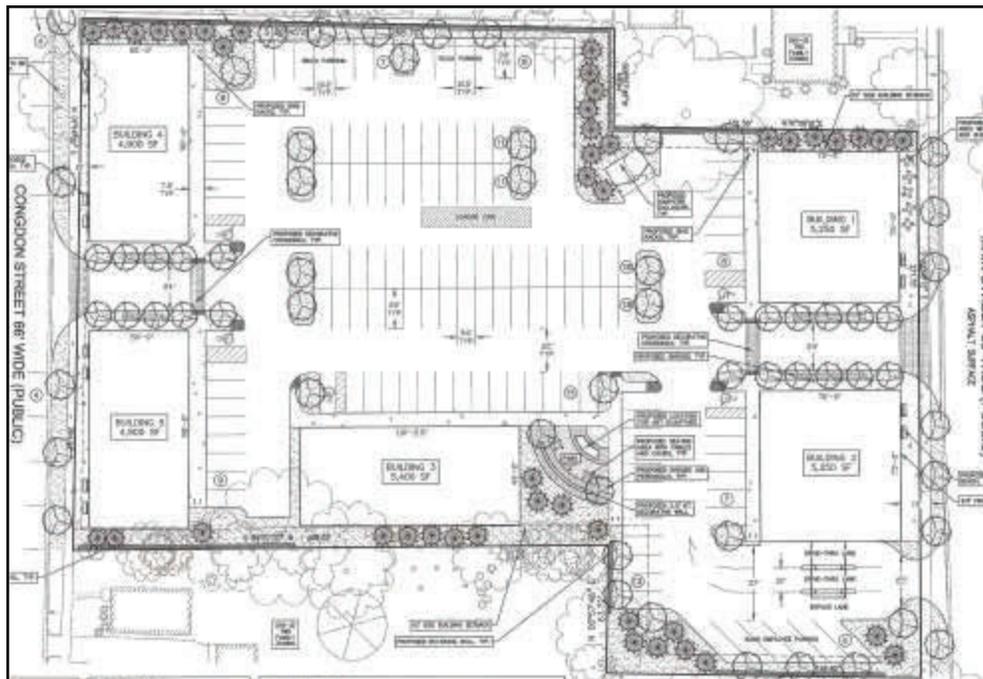
F. Creating Neighborhood Civic/Commercial Centers

Intent

To conveniently concentrate services and activities to serve multiple residential clusters.

Guidelines

1. Civic/Commercial Centers shall be located to serve groupings of clusters as well as pass-by traffic in order to support an array of shops and services.
2. Such centers shall be anchored by a public green space and, ideally, a public building such as a school or meeting hall.
3. Upper story housing above retail or commercial space is strongly encouraged within Civic/Commercial Centers.



General Principles & Site Planning

G. Interface with Adjoining Development

Intent

To ensure a transition in development intensity at the perimeter of MPD projects.

Guidelines

1. Where individual lot residential development is located along the boundary of an MPD, lot sizes shall be no less than 75% the size of the abutting residential zone or 7200 sq. feet, whatever is less.
2. Multi-family and non-residential land uses should include a minimum 25 ft wide dense vegetative buffer when located along the boundary of an MPD.
3. When there is no intervening development proposed, a minimum 25 ft wide dense vegetative buffer should be provided between main entrance or access routes into an MPD and any adjoining residential development.

A. Streets*Intent*

To establish a safe, efficient and attractive street network that supports multiple choices of circulation, including walking, biking, transit and motor vehicles.

Guidelines

1. Connectivity
 - a. The street layout shall create a network that promotes convenient and efficient traffic circulation and is well connected to other existing City streets.
2. Design
 - a. The layout of streets should relate to a community-wide focal point.
 - b. A consistent overall landscape theme should be utilized, with variations provided to indicate passage through areas of different use, densities, topography, etc.
 - c. Limit the use of backyard fences or solid walls along arterial streets.
3. Reduced Pavement Widths
 - a. Pavement widths should be minimized to slow vehicular speeds and maintain an area friendly to pedestrians and non-motorized users.
4. Low-Impact Design
 - a. Stormwater runoff should be reduced through “natural” techniques: flush curbs, bio-filtration swales, use of drought-tolerant vegetation within medians and planting strips, etc.
5. Traffic calming methods should include:
 - Roundabouts
 - Traffic Circles
 - Chicanes
 - Corner bulbs
6. Lanes and Alleys
 - a. Access to rear residential garages and commercial loading and service areas shall be available through lanes and alleys.
7. Non-motorized Circulation
 - a. All streets shall include either sidewalks or trails on at least one side of the street. Design streets to be “bicycle” friendly.
8. Street Landscaping
 - a. All streets shall include native and/or drought-tolerant vegetation (trees, shrubs and groundcover) planted within a strip abutting the curb or edge of pavement. Native and/or drought-tolerant vegetation shall also be used within all medians.
9. On-Street Parking
 - a. Curbside parallel parking shall be included along residential streets. Parallel or angle parking should be included within non-residential areas.

Circulation



B. Sidewalks

Intent

To provide safe, continuous pedestrian linkages within the street right-of-way.

Guidelines

1. Width
 - a. The minimum clear pathway shall generally be between 5 ft and 8 ft, depending upon adjacent land uses and anticipated activity levels.
2. Lighting
 - a. All lighting shall be shielded from the sky and surrounding development and shall be of a consistent design throughout various clusters of the development.
3. Furnishings
 - a. Street furnishings including seating, bike racks, and waste receptacles shall be located along main streets in Civic/Commercial areas.
 - b. Furnishings serving specific businesses (outdoor seating) will require a building setback and shall maintain a minimum passable width of the sidewalk.
 - c. Mailbox stations shall be designed to be architecturally compatible with the development in which they are located.



C. Walkways and Trails

Intent

To provide safe, continuous pedestrian linkages throughout and sensitive to the project site, open to both the public and project residents.

Guidelines

1. Location
 - a. Walkways and trails shall be integrated with the overall open space network as well as provide access from individual properties. Trail routes shall lead to major community activity centers such as schools, parks and shopping areas.
2. Width
 - a. Not less than 8 feet wide to allow for multiple modes of use.
3. Materials
 - a. Walkways connecting buildings and hardscaped common spaces shall have a paved surface.
 - b. Trails throughout the development and connecting to larger landscaped common spaces shall be of at least a semi-permeable material.



Site Design

A. Cluster Development

Intent

To ensure that development is compatible with the small town character currently found within Black Diamond.

Guidelines

1. Larger groupings of development should be divided into smaller neighborhood clusters of approximately 50 dwelling units that are defined by open space.
2. Clustering
Within projects, higher density residential development shall be designed to have a village-like configuration. This includes elements such as:
 - a. Houses of varying sizes, styles, and form;
 - b. The maximum number of attached units shall not be more than twelve within a single structure.



Site Design

B. Neighborhood Common Space

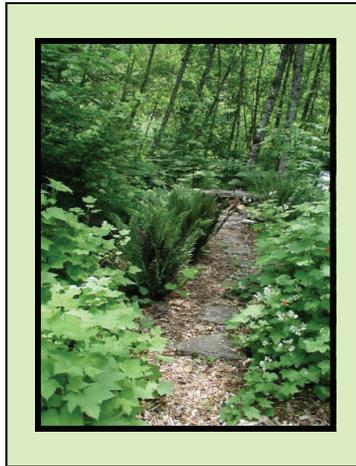
Intent

To provide a variety of usable and interesting open space that supports an active community.

Guidelines

1. Amount
 - a. In general, within higher density residential and commercial development, a minimum of 1% of the lot area plus 1% of the building area should be the amount of area set aside for common space, exclusive of other required landscaping.
2. Location
 - a. Common open space shall be accessible and visible to users, as well as integrated into the overall project through connections and trails.
3. Landscaping/Hardscaping
 - a. Commercial areas shall provide common space in the form of plazas, courtyards, and/or seating areas including some of the additional features noted below.
 - b. Higher density residential areas shall have usable outdoor spaces that provide at least four of the following features to accommodate a variety of ages and activities:
 - Site furnishings (benches, tables)
 - Picnic areas
 - Patios or courtyards
 - Gardens
 - Open lawn with trees
 - Playfields
 - Special interest landscape
 - Public art
 - Water features
 - Sports courts (tennis, basketball, volleyball)
4. Lighting
 - a. Pedestrian scale, bollard, or other accent lighting may be incorporated into the design of open space.





C. Landscaping & Planting Design

Intent

To provide well-designed public parks and greens within the development.

Guidelines

1. Incorporate native, drought-tolerant vegetation, avoid extensive use of lawn and plantings that demand significant irrigation and fertilization.
2. A minimum of 75% of the landscaped area (not including recreational areas) should be planted with other than turf or lawn. Perennials and annuals are encouraged to provide special interest and highlight pedestrian areas such as walkways and trails.
3. Where landscape areas are located adjacent to a street right-of-way, the type of landscaping should provide a vertical buffer.
4. Rocks, pebbles, sand, and similar non-living materials shall not be used as groundcover substitutes, but may be used as accent features provided such features do not exceed a maximum 5% of the total landscape area.

D. Stormwater Detention/Retention Ponds

Intent

To integrate stormwater facilities as project amenities.

Guidelines

1. Location
 - a. Use natural site topography plus low-impact development methods to determine appropriate locations, which is to be integrated into the overall project design.
2. Landscaping
 - a. Where possible, provide facilities that are site amenities, in order to reduce need for fencing. In general, public access to stormwater facilities should be included within design.
3. Fencing
 - a. Chain link fencing shall not be allowed. Other forms of non-obscuring fencing may be permitted when ponds exceed a safe slope. However, it is generally expected that ponds will be gently integrated into the design of the site with slopes that are safe to traverse on foot (less than 7% grade).

Building Design

17

A. Residential Building Design

Intent

To ensure that new development complements and strengthens the character of Black Diamond and to allow for maximum flexibility in location, size and configuration of houses while ensuring that residential structures are in scale with lot sizes.

Guidelines

1. Variety of Styles
 - a. Provide a variety of building solutions through the mixing of one and two story building profiles. Limit the amount of replication of building styles within one block.
2. Setbacks of Houses to Create a Sociable Environment
 - a. The front facades of houses should be setback between 5 and 15 feet from the back of the sidewalk. Vary front and side yard setbacks from house to house to provide interest and variety.
3. Setbacks of Garage to Reduce Visual Impact
 - a. The preferred location for garages is at the rear of the lot, with vehicular access being provided from an alley. Garage doors should be within 10 ft of the alley.
 - b. If alley access is not possible, then garages shall be setback at least 20 ft from back of the sidewalk. That distance can be reduced when garage doors do not face the street.
4. Architectural Features
 - a. Housing shall include features such as:
 - Dormers
 - Brackets supporting roof overhangs
 - Corner boards
 - Wide trim around windows
 - Railings around balconies and porches
 - Low picket fencing
 - b. Fronts of houses shall face the street and incorporate usable porches, stoops and steps.
 - c. Upper floors of houses shall be smaller than the floors below.
 - d. Orientation of ridgelines of homes shall be varied.
5. Materials
 - a. Exterior finishes should incorporate traditional and natural building materials as historically used in Black Diamond.
6. Floor Area Ratio (FAR) (Building size to lot size)
 - a. FAR for detached residential development should not exceed 0.75;
 - b. Attached forms of residential may be up to 1.0 FAR;
 - c. Within Commercial/Civic Centers, residential development FAR may be as high as 2.5
7. Height
 - a. Minimum 1 story above grade
 - b. Maximum 2 1/2 stories
8. Massing
 - a. Horizontal facades longer than 30' shall be articulated into smaller units, using methods such as:
 - distinctive roof forms
 - changes in materials and/or patterns
 - color differentiation
 - recesses or offsets.

A. Residential Building Design Guidelines CONTINUED

9. Roof Pitch
 - a. May range from 6:12 to 12:12
10. Architectural Features
 - a. Front Porches—at least 6 ft in depth (or deep enough to allow for seating)
 - b. Street-Facing Garage Location—the main house floor area shall extend at least 5 ft closer to the front lot line than any garage with street-facing doors. Design measures should be used for de-emphasizing garages, such as:
 - porches
 - trellises
 - location of entry
 - break up massing/doors for double garages
 - overhanging second floor

NON-RESIDENTIAL BUILDINGS SHALL BE SUBJECT TO APPLICABLE CITY OF BLACK DIAMOND DESIGN GUIDELINES.

BLACK DIAMOND DESIGN GUIDELINES
for
BUSINESS PARK/INDUSTRIAL AREAS



Adopted June 18, 2009

Introduction and Purpose

This section of the Design Standards and Guidelines focuses on site planning and design guidance for the Business Park/Industrial Park and Industrial Zones. Details on site design and building design are included in these guidelines to promote sensitive site planning and low impact development to preserve significant natural features and overall community character.

Site Design

Emphasis on the siting of buildings should be placed on clustering buildings and parking to preserve open space and significant natural features as community amenities, and to take advantage of opportunities to reflect and express the community's semi-rural character through the arrangement of buildings and landscape.

Building Design

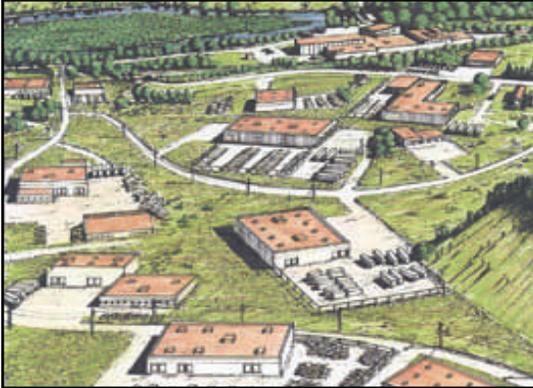
Typically, office campuses present a unified character to lend brand identity, professionalism and credibility. These guidelines stress a semi-rural, contextual approach to this development pattern with the following design directives:

- acknowledge and respect local natural features;
- design for compatibility with adjacent uses;
- design for overall cohesiveness;
- consider each building as a high-quality, long term addition to the city

SITE DESIGN

Over-arching Site Design Intent

Emphasis on the siting of buildings within a business park should be placed on clustering buildings and parking to preserve open space and significant natural features as community amenities, and to take advantage of opportunities to reflect and express the community's semi-rural character through the arrangement of buildings and landscape. Site design should also recognize and relate to adjacent streets and adjacent developments.



Sprawling development patterns tend to ignore natural features and can negatively impact neighboring uses.



Integrated development can preserve and take advantage of unique natural features by clustering buildings, parking, access and circulation.



A. Integrated Site Planning

Intent

Each component of a business park and industrial campus should contribute to a coherent sense of the whole, including:

- encouraging cluster development to preserve open space and significant natural features as community assets;
- discouraging developments that result in “left-over” open space; and
- recognizing the important relationship between private, semi-public and public spaces in the arrangement of buildings, parking, service access, and pedestrian areas.

Guidelines

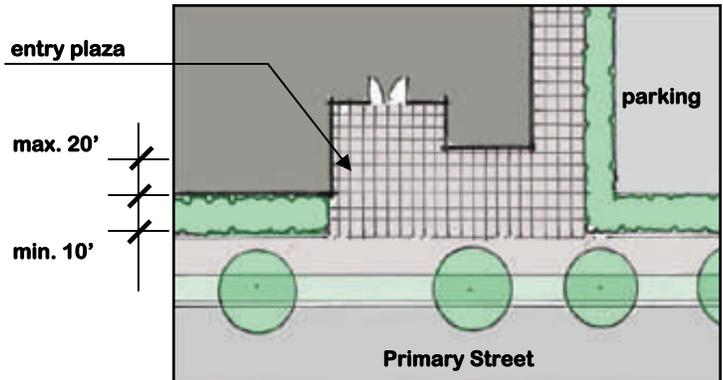
1. Preserve natural features (wooded areas, knolls, ponds and streams) and community landmarks (significant trees, farmhouses) by concentrating and clustering buildings on the land of least natural sensitivity/significance.
2. Establish clear pedestrian connections on site that are well-marked and ADA-compliant. Pedestrian paths or walkways should connect to all businesses and building entries, and through parking lots to direct pedestrians to buildings, streets and public spaces.
3. Discourage buildings that are internally focused at the expense of the pedestrian environment.
4. Buffer adjacent sensitive land uses from undesirable impacts that may originate from the site; buffers may be landscape and/or architectural in character.

SITE DESIGN

B. Building Orientation: Street Front

Intent

Buildings should be oriented with consideration to the visual impact from the perspective of the driver or pedestrian on the primary roadway. It is important to create the immediate first impression of a high quality business park, as part of the city's overall community character and identity.



Building orientation and setbacks on primary street

Standards

1. Sidewalks shall be provided on primary streets with the following dimensions:
 - a. Sidewalk area with a clear zone of 5 feet for pedestrian travel;
 - b. A 4-foot-wide continuous planted area.
2. Buildings shall be developed with "principal" facades on the primary street. Front setbacks shall be a minimum 10 feet and a maximum 20 feet from the front property line. Establish a streetscape defined by attractive buildings located near the street edge within a landscape setting.

Guidelines

1. Buildings should be sited in ways which make their entries or intended use clear to approaching visitors. Sole building entries from parking lots are discouraged.
2. Set buildings back at the corners of intersections from the corner property lines to allow for more generous sidewalk, additional street landscaping, and business signage.
3. Front internal access drives with a combination of buildings and landscaping and served by pedestrian walkways.

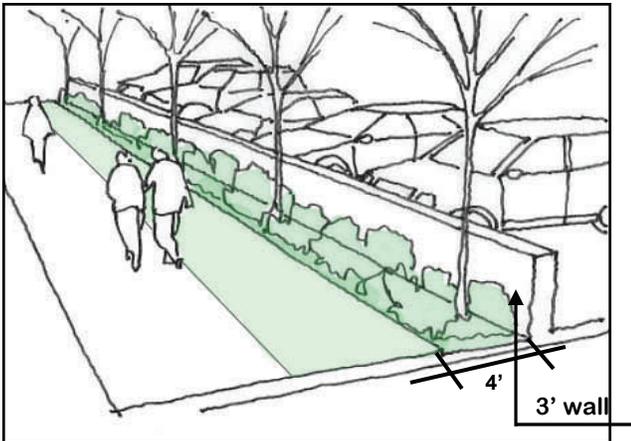


Walkways, planting strips and a variety of trees make internal drives more visually appealing and safer for pedestrians

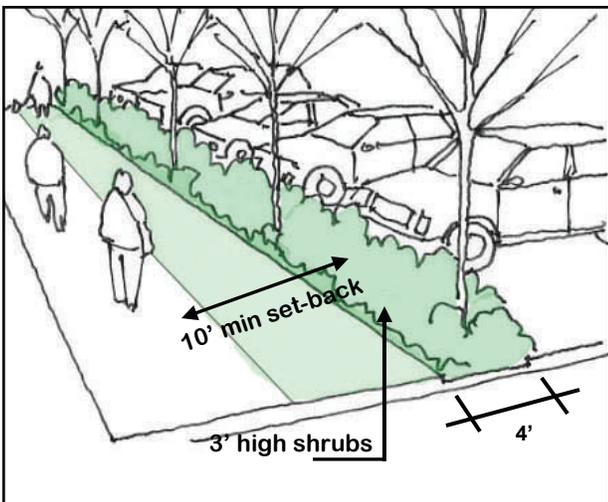
SITE DESIGN



Low masonry wall with landscaping



Surface parking from public view with wall and vegetation



Surface parking from public view with vegetation

C. Parking Lot Screening

Intent

To reduce the impacts of surface parking while also maintaining visibility for surveillance.

Standards

1. Portions of parking lots that are in front of or beside buildings along the primary street(s) from the right of way shall be screened with one or more of the following treatments:
 - a. Landscaping to sufficiently screen the first 3 feet in height adjacent to the parking area, within a 4 feet planting area.
 - b. Low walls made of concrete, masonry, or other similar material and not exceeding a height of 3 feet. Ground cover and trees must still be provided in the required 4 feet planting area.
 - c. Raised planter walls planted with a minimum 80% evergreen shrubs not to exceed 3 feet in height.

SITE DESIGN

D. Parking Lot Location and Landscape

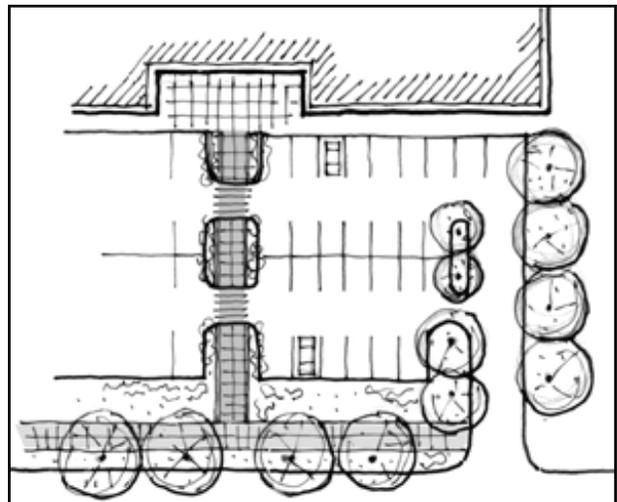
Intent

To reduce the visual impact of parking lots through landscaped areas that complement the overall design and character of development, providing both aesthetic and environmental benefit.



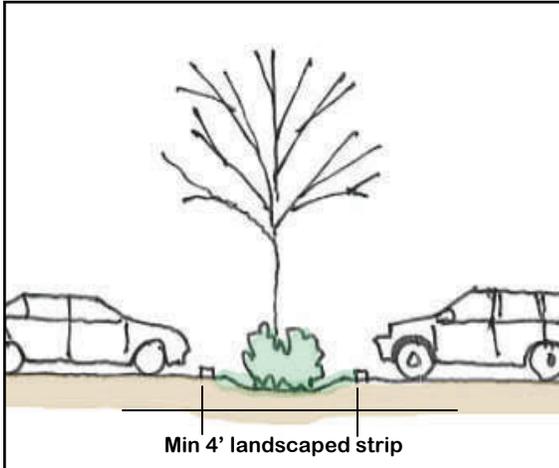
Standards

1. Locate parking lots to the side or behind buildings when possible. Parking located on the side or in front of the building shall meet the following:
 - a. Parking is set back a minimum of 10 feet from the property line;
 - b. Surface parking areas include 5 feet of perimeter landscaping and meet Guideline B when facing a Primary Street(s);
 - c. Parking area does not exceed 50% of the total frontage along the Primary Street(s);
 - d. Promote shared parking access and shared parking among adjacent businesses.
2. Parking lots shall not abut street intersections (corner lots).

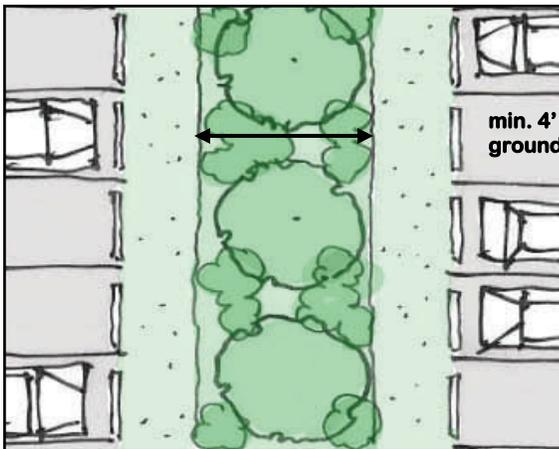


Screen parking from all neighboring properties and public rights-of-way with perimeter landscaping

SITE DESIGN



Interior parking lot landscaping strips



Interior parking lot landscaping strips, plan view



Landscaped 'fingers' break up the expanse of asphalt

D. Parking Lot Location and Landscape Standards Continued

3. Dedicate 10% of all parking areas to interior landscaping, including the following:
 - a. Interior landscaping shall be provided every 16 parking stalls and at the ends of each row of parking;
 - b. Landscaping strips shall be a minimum of 4 feet wide and consist of ground cover, drought tolerant shrubs at a rate of 1 shrub per space, and at least one tree per every 6 spaces.
4. The use of pervious pavers and other low impact methods of stormwater runoff infiltration in the design of parking areas shall count towards the 10% interior landscaping requirement.

SITE DESIGN

E. Pedestrian Connections

Intent

Business parks and campuses are enhanced when it is easier, safer, and more comfortable to walk between buildings and parking areas.

New developments should connect to surrounding uses, including streets, businesses, transit facilities, and residential areas, where appropriate.

Standards

1. Parking lots greater than 25 stalls shall include a pedestrian walkway. Separate pathways through parking lots from vehicle parking and travel lanes by a combination of 2 or more of the following ways:
 - a. A 6 inch vertical curb in combination with a raised walkway.
 - b. a trellis, special railing, bollards, and/or other architectural features to accent the walkway at key points.
 - c. a continuous landscape area minimum 3 feet wide on at least one side of the walkway

Guidelines

1. Use pathways to connect open spaces within the business park or campus, and link to adjacent trails, where appropriate. Use way-finding elements, including signage and paving inlays to direct visitors to specific businesses and buildings within a park or campus.
2. Reinforce pedestrian connections with pedestrian scale lighting for safety. Pedestrian scale lighting should be a maximum of 16 feet in height.



Pedestrian connections provide safe and comfortable access within a campus



Connections can be naturalistic in character

SITE DESIGN



F. Site Lighting

Intent

Lighting is an important aspect of overall site development as it contributes to a feeling of safety and consistent “site vocabulary” to create a campus identity.

Standards

1. Short masted and local area lighting are the preferred lighting standards. Short masted lights shall not exceed a maximum of 16 feet in height. Parking lot lighting shall not exceed 30 feet in height.
2. Mount site lighting required for loading, staging and storage areas on the building facades if facing towards the site. The lighting shall not be oriented towards any public right-of-way or adjacent properties.



Guidelines

1. Consider using bollard lighting for pedestrian pathways and sidewalks.



Various lighting methods including bollard lighting for pedestrian walkways

SITE DESIGN

G. Screening of Trash, Loading and Service Areas

Intent

To reduce the impact of service, loading, and trash/recycling areas. An important objective of the Business Park and Industrial design guidelines is to minimize clutter that can be associated with these uses.



Standards

1. Service, loading and trash/recycling collection areas shall be screened from public view with solid evergreen plant material or architectural treatment similar to the design of the adjacent building.
2. Loading and service areas shall not face any residential district, unless no other location is possible.
3. Outdoor storage areas shall be consolidated into a single area, and screened from the street and/or neighboring uses by buildings and/or landscaping.



Examples of landscaping screening methods

BUILDING DESIGN

Over-arching Building Design Intent

A clear visual and functional relationship between buildings, grounds, security and layout creates a cohesive campus. These guidelines emphasize a consistent “site vocabulary” to unify the site with similar signage and graphics, hierarchical paving, site furniture and landscaping. Within this concept, design emphasis is placed on building massing to complement the site furnishings, while distinctive design treatments in individual buildings can add three-dimensional quality and variety in character.



Example of simple modulation with deep openings that create shadow lines, provide visual relief that is carried to the top of the building.



Semi-rural character expressed in the form and roof line of a commercial building to help reduce apparent bulk

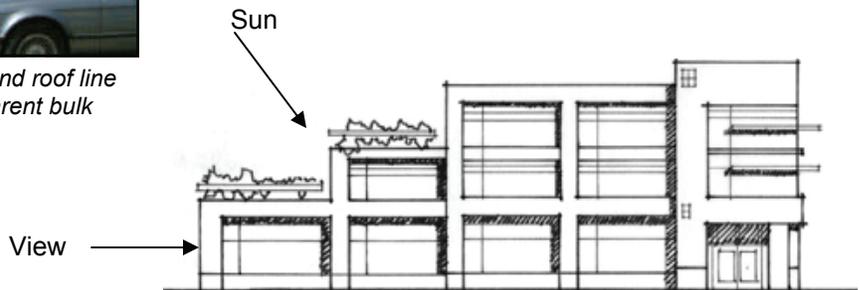
A. Massing and Bulk

Intent

The form and articulation of Business Park and Industrial buildings are expected to contribute to the desired campus environment. Large, monolithic buildings are discouraged.

Guidelines

1. Break down the scale and massing of larger buildings by modulating the building, both horizontally and vertically into smaller scale volumes. Carry this modulation up to the roofline.
2. Use floor to floor terracing on multi-story buildings to reduce bulk and increase amenity space as well as views.
3. Step buildings down towards surrounding residential uses and orient massing towards taller buildings nearby.
4. Design roofs to complement the composition and form of the building and the surrounding area. Gable or hipped roofs should be used if interpreted in a manner appropriate for an office or industrial use (such as metal materials). Use a strong, detailed cornice or parapet in conjunction with a flat roof. Employ high quality roofing material for roofs that are visible from the street.



Terracing provides visual relief and amenity

BUILDING DESIGN

B. Form and Articulation

Intent

Reduce the apparent bulk of multi-story buildings and add richness and variety to Business Park/Industrial Campus

Guidelines

1. Maintain similar style, materials and scale with surrounding buildings. Use regulating lines such as soldier courses and rhythms like columns to break up long facades.
2. Provide a clear pattern of building openings. Windows, doors and other openings should unify a building's facade and add considerably to the facade's three dimensional quality.



Unique architectural articulation reduces the perception of building bulk



The composition of openings (windows and doors), pilasters and horizontal bands and cornice lines helps achieve a human scale.

BUILDING DESIGN



Double height glazing, columns and landscaping give prominence and identity to a main building entrance

C. Building Entrances

Intent

Provide well-marked, articulated entrances oriented to public spaces.

Standards

1. Main building entrances shall be oriented on either the primary street or main internal vehicle drive.



Separate entrance structures increase the prominence of building entrances

Guidelines

1. Accentuate the entrance(s) to a building's main lobby or interior office space; these main building entrances should be prominent in terms of size, articulation and use of materials.
2. Encourage the use of highly crafted materials or civic art pieces to further enhance the appearance and prominence of entrances.

BUILDING DESIGN

A. Sign Design

Intent

To ensure that business park and industrial campus signs are not solely oriented to automobile traffic.



Standards

1. Pole signs are prohibited. Ground signs shall be no higher than 6 feet, and an integral part of the development's architectural design.
2. The base of any ground sign shall be planted with shrubs and seasonal flowers.



Various sign styles that cater to pedestrians and motor vehicles